

MY NAME IS GREG VAN GRINSVEN, AND ON BEHALF OF YOUR TOWN ROAD IMPROVEMENT PROJECT COMMITTEE, I WOULD LIKE TO WELCOME YOU TO THE FIRST OF FOUR PLANNED PUBLIC INFORMATION MEETINGS.

WHAT MOST OF YOU ARE HOLDING IN YOUR HANDS IS THE CURRENT OUTPUT OF THE PROCESS WE ARE USING TO CREATE YOUR TOWN ROAD IMPROVEMENT PLAN.

WHILE IT IS NOT A FINISHED DOCUMENT, IT IS A VERY GOOD REPRESENTATION OF WHAT WE AS A COMMITTEE SEE AS THE POSSIBLE SCENARIOS THAT CAN BE PURSUED TO IMPROVE OUR ROADS AND THEIR ASSOCIATED INVESTMENT COSTS.

IT WILL BE MY MISSION TO FILL YOU IN ON HOW THESE DOCUMENTS CAME INTO BEING, WHAT THEY MEAN AND WHAT WE ARE GOING TO DO TO CREATE A FINAL RECOMMENDATION.

AT THE END OF THE DAY THIS IS YOUR PROJECT AND IT IS OUR GOAL TO MAKE SURE YOU TO HAVE ALL THE INFORMATION YOU WILL NEED TO MAKE AN INFORMED DECISION AS TO ITS OUTCOME.

I PLAN TO TAKE ABOUT AN HOUR TO COMPLETE MY UPDATE AND THEN IT WILL BE YOUR TURN TO TALK

JUST A HEADS UP, I AM GOING TO BE THROWING A LOT OF INFORMATION AT YOU IN A VERY SHORT PERIOD OF TIME. I WOULD PREFER THAT YOU HELD YOUR QUESTIONS TO THE END, BUT IF I COMPLETELY LOSE ANY OF YOU ALONG THE WAY PLEASE STOP ME AND I'LL TRY AND GET US BACK ON TRACK.

## **PUBLIC INFORMATION MEETING**

- **Key Messages**
  - Information Sharing
  - Information Gathering
  - Not a Decision Making Meeting

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IN ORDER TO SET THE PROPER CONTEXT FOR THIS MEETING, WE AS A COMMITTEE HAVE ESTABLISHED THREE KEY MESSAGES WE'D LIKE TO LEAVE WITH YOU.

THIS MEETING IS ABOUT BOTH INFORMATION SHARING AND INFORMATION GATHERING

**CLICK**

IT IS NOT, HOWEVER, A DECISION MAKING MEETING.

WHILE THERE HAS BEEN A LOT OF WORK DONE TO GET TO TODAY'S MEETING, A LOT MORE WORK REMAINS TO BE DONE BEFORE A FINAL DECISION IS MADE.

MORE ON THAN IN A MINUTE

## PUBLIC INFORMATION MEETING AGENDA

- **Process**
- **Project Summary**
- **Survey Overview**
  - Priority Ranking
  - Costs Estimates
- **Project Scenarios**
- **Q&A Feedback Session**
- **Information Breakout Session & Adjournment**

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Using the following agenda, I would now like to take you on a deeper dive into the project details.

The first four agenda items are tied to information sharing

And the last two agenda items tied to information gathering. **Click**

The first agenda item is a review of the process we are using to create the road improvement plan. **Click**

The second agenda item is a review of the project summary. This is the same summary on page one of your handout. **Click**

The third agenda item is an overview of the survey

It will start with a high level review of the road survey

Followed by a quick review the current cost estimates for the project **click**

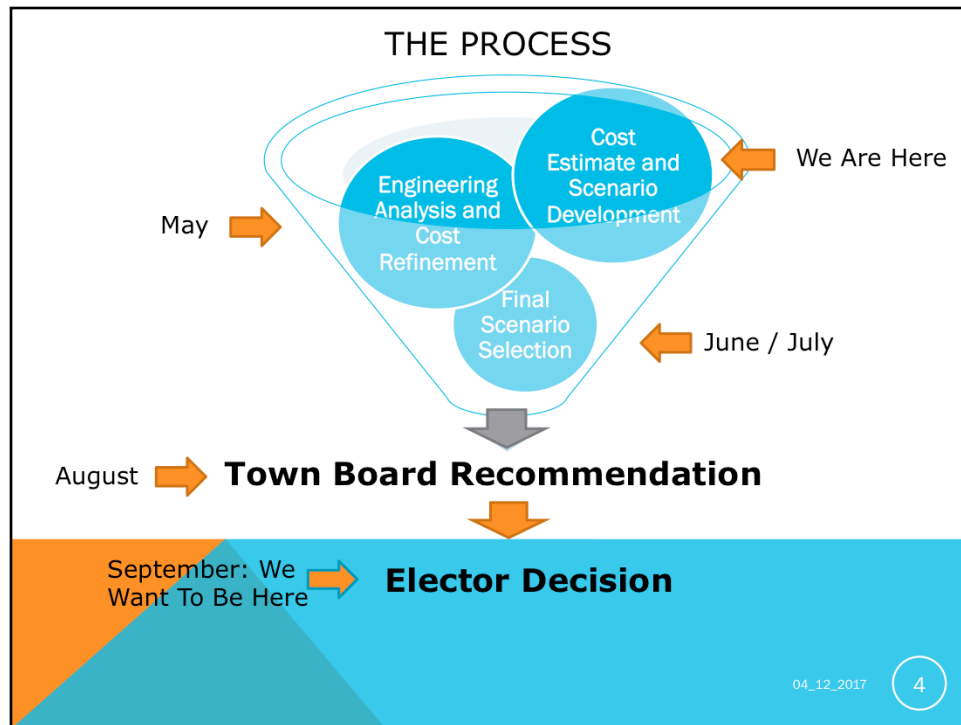
The fourth agenda item is a review of the project scenarios. They are the second part of your handout. **Click**

The fifth and sixth agenda item are citizen feedback sessions.

This is where I will stop talking and we as a committee will start listening. **Click**

The fifth one is Q&A session and

Once it is over, we breaking from the formal part of the meeting to an informal part where we'll be inviting you to stop and talk to members of the committee to learn more about any one or all seven scenarios currently under review.



## THE PROCESS

THE FUNNEL DIAGRAM IN FRONT OF YOU IS INTENDED TO GIVE YOU AN OVERALL PERSPECTIVE OF THE PROCESS WE AS A COMMITTEE ARE USING TO CREATE THE PROJECT PLAN.

THE FIRST STEP OF THE PROCESS IS DATA GATHERING AND COST ESTIMATE CREATION. WE AS A COMMITTEE HAVE COMPLETED THIS FIRST STEP AND ITS OUTPUT, THE DOCUMENTS YOU HOLD IN YOUR HAND, WILL BE THE FOCUS OF THIS PRESENTATION. **CLICK**

IN MAY, WE HOPE TO COMPLETE A FORMAL “CONDITIONS ASSESSMENT” OF EACH ROAD. WITH THE HELP OF AN ENGINEERING FIRM, EVERY ROAD WILL BE ASSESSED FROM AN ENGINEERING PERSPECTIVE TO OUTLINE THE SPECIFIC REQUIREMENTS NEEDED TO RECONSTRUCT IT. THE OUTPUT OF THIS ASSESSMENT WILL ALLOW THE COMMITTEE TO BETTER ASSESS EACH OF THE SCENARIOS FOR THEIR VALIDITY AS WELL AS ALLOWING US TO FURTHER REFINE OUR COST ESTIMATES. **CLICK**

IN JUNE AND / OR JULY WE WILL BE UTILIZING THE FEEDBACK FROM THE FOUR INFORMATION SESSIONS, AS WELL AS THE REFINED COST INFORMATION, TO SELECT THE ONE SCENARIO WE WILL BE RECOMMENDING TO THE TOWN BOARD FOR ITS REVIEW AND POSSIBLE APPROVAL. **CLICK**

IN AUGUST THE COMMITTEE WILL FORMALLY PRESENT ITS RECOMMENDATION TO THE TOWN BOARD, AND **CLICK**

IN SEPTEMBER WE HOPE TO BE BACK IN FRONT OF YOU WITH A RECOMMENDATION FOR YOU TO VOTE ON.

# Project Summary

**GOAL:**

Develop a long-term road improvement plan to maintain, reconstruct where necessary, and guide the construction of new roads for the Town of Boulder Junction that is acceptable to the electorate.

**BACKGROUND:**

There are 64 miles (71%) of the town's 93 total miles of roads that have had little or no major reconstruction in recent history and are in need of repair.

Without a plan to properly reconstruct and maintain the town's roads, deterioration will continue and the costs of reconstruction will continue to rise.

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THE SECOND AGENDA ITEM IS A REVIEW OF **THE PROJECT SUMMARY** WHICH IS THE FRONT PAGE OF THE HANDOUT YOU WERE PROVIDED WHEN YOU ENTERED THE ROOM.

I WANT TO SPEND A COUPLE OF MINUTES REVIEWING THIS DOCUMENT BECAUSE WE AS A COMMITTEE ARE USING IT TO GUIDING THE CREATION OF OUR ROAD IMPROVEMENT PLAN.

THERE ARE THREE COMPONENTS OF THE PROJECT SUMMARY. THEY ARE:

THE GOAL, BACKGROUND AND GUIDING PRINCIPLES

READ THE SLIDE

**CLICK**

# Project Summary

## **GUIDING PRINCIPLES:**

- Grants and cost sharing by other entities will be pursued to offset any approved investment level.
- All bid documents will include the appropriate levels of engineering specifications to promote competition and cost effectiveness.
- Proper drainage determines the longevity of any road surface making it a key driver in selecting the appropriate surface material for any given road.
- Any road reconstruction plan needs to include an appropriate level of additional funding for long-term maintenance.

READ THE SLIDE

CLICK

## Survey Overview

### 2015 Town Road Survey

- Priority Ranking
- Cost Estimates

**NOTE:**

- Any information being shared is available in the Town Office and posted on the town website; [www.townofboulderjunction.org](http://www.townofboulderjunction.org).  
Subscribe to get automatic updates.

Demo Active Board

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THE THIRD AGENDA ITEM IS AN OVERVIEW OF THE ROAD SURVEY THAT WAS CONDUCTED AT THE END OF 2015

**THIS SURVEY IS A CRITICAL PART OF THE OVERALL PROJECT PLAN AS IT REPRESENTS THE FOUNDATION ON WHICH ALL OF THE SCENARIOS WERE BUILT. REPEAT TO EMPHASIS ITS IMPORTANCE. AND THESE SCENARIOS ARE THE SECOND PART OF YOUR HAND OUT AND THE SUBJECT OF A DEEPER DIVE IN THE NEXT SECTION OF THE AGENDA.**

IN MY OVERVIEW I WILL BE SHOWING YOU FIRST HOW THE ROADS WERE RANKED AND SECONDLY HOW THE COST ESTIMATES FOR THEIR RECONSTRUCTION WERE CREATED.

BEFORE I START, HOWEVER, I WOULD LIKE TO MAKE YOU AWARE THAT ALL OF THE INFORMATION RELATED TO THIS PROJECT IS AVAILABLE IN HARD COPY IN THE TOWN OFFICE OR ON LINE ON ITS WEBSITE.

**CLICK**

ADDITIONALLY, I WOULD LIKE TO USE THE "SMART BOARD" IN FRONT OF THE ROOM TO QUICKLY SHOW YOU THE CONTENTS OF THE ENTIRE SURVEY AS MY POWERPOINT PRESENTATION CONTAINS ONLY SELECTED SCREEN SHOTS OF THE SURVEY RESULTS

**CLICK**

DEMO THE ACTIVE BOARD – HIGHLIGHT THE KEY AND PRIORITY RANKING WORKSHEET

**CLICK**

# Survey Overview

## 2015 Town Road Survey:

- Priority Ranking

SAFETY  
Traffic  
Occupancy  
Condition

PLAN CAT	ROAD NAME	Length		SURFACE TYPE			RATING				RESURFACE PLAN			RESURFACE COST EST
		Miles	Accum.	Gravel	Chip	Asphalt	Traffic	Occupancy	Condition	Priority	Gravel	Chip	Asphalt	
3	Allen Road	0.68	0.68		0.68		5	5	5	15		0.00	0.68	\$220,320
3	Fishtrap Lake Road	2.04	2.72		2.04		5	5	5	15		0.64	1.40	\$340,800
3	Newcomb Lane	3.50	6.22		3.50		5	5	5	15			3.50	\$756,000
3	North Creek Road	4.57	10.79		4.57		5	5	5	15			4.57	\$987,120
3	Fallon Road	2.08	12.87		2.08		5	5	4	14			2.08	\$449,280
2	Island Lake Road	1.28	14.15		1.28		4	5	4	13		1.28		\$121,600
2	High-Fishtrap Lake Road	1.10	15.25		1.10		5	5	3	13			1.10	\$356,400
4	Big Muskellunge Road	2.70	17.95	1.20	1.00	0.50	5	3	4	12		2.20		\$167,000
2	Kern Lane	1.10	19.05		1.10		3	4	5	12		1.10		\$104,500
4	Airport Road (paved)	2.45	21.50			1.45	5	5	2	12			0.00	\$0
2	Shamrock Lane	0.40	21.90		0.40		3	4	4	11		0.40		\$38,000
2	Concora Road	0.80	22.70		0.80		3	3	5	11		0.80		\$76,000
2	High Lake Road	3.80	26.50		1.30		3	3	5	11		1.10	0.70	\$331,300
2	Old K Road	2.79	29.29		2.79		3	3	5	11		2.79		\$397,575
3	Dairymens Road	1.55	30.84		1.55		4	5	2	11			1.55	\$502,200
1	Oswego-Fishtrap Lake Road	1.90	32.74		1.90		4	5	2	11		0.00		\$0
1	South Town Line	1.36	34.10		1.36		4	5	2	11		0.00		\$0
2	Bakken Road (East)	1.07	35.17		1.07		3	3	4	10		1.07		\$101,650
3	Highland Drive	0.28	35.45		0.28		3	3	4	10			0.28	\$60,480
2	Rudolph Lake Lane	0.60	36.05		0.60		3	4	3	10		0.60		\$57,000
2	Wool Lake Lane	0.76	36.81		0.76		3	3	4	10			0.76	\$72,200
1	North Town Line Rd (Town Road 116)	1.00	37.81		1.00		4	4	2	10		0.00		\$0
2	Harmony Point Lane	0.60	38.41		0.60		2	3	5	10		0.60		\$57,000
2	Shore Drive	1.03	39.44		1.03		2	3	5	10			1.03	\$97,850

As I mentioned the actual survey was completed at the end of 2015 during which the three members of the town board drove all 94 miles of our town roads and assessed each road based upon three criteria:

- SAFETY
- TRAFFIC LEVEL,
- OCCUPANCY LEVEL,
- AND CONDITION.

In this screen shot you can see the ranking of the first 38 miles of roads, and their ratings in descending order from 15 to 10..

I would encourage everyone to read this document from the perspective of its priority ranking in comparison to the other roads on the list and not from the perspective that Allen road is the number one road because it is at the top of the list.

Allen road is just one of 4 four roads receiving a score of 15 just as old k road is just one of the seven roads receiving a score of 11.

It is also likely the sequencing of what roads get fixed in what order may change based upon actual bids because it may be much more cost effective to group roads together based upon their geographic relationship to each other rather than their listed order of priority ranking.

It is, as stated, a very good starting point to determine the priority grouping and the associated costs of reconstruction of each road on the list.

Process check: are we all still on the same page?????

Click



# Survey Overview

## 2015 Town Road Survey:

- Cost Estimates

### PAVING COST PER MILE

		NEW Surface		
EXISTING Surface	GRAVEL	CHIP SEAL	ASPHALT	
GRAVEL	\$ 40,000	\$ 60,000	\$ 197,000	
CHIP SEAL	\$ 40,000	\$ 95,000	\$ 216,000	
ASPHALT	\$ 40,000	\$ 95,000	\$ 216,000	

PLAN CAT	ROAD NAME	Length		SURFACE TYPE			RATING				RESURFACE PLAN			RESURFACE COST EST		ACCUM. COST EST
		Miles	Accum.	Gravel	Chip	Asphalt	Traffic	Occupancy	Condition	Priority	Gravel	Chip	Asphalt			
3	Allen Road	0.68	0.68	0.68			5	5	5	15		0.00	0.68	\$220,320	\$220,320	
3	Fishtrap Lake Road	2.04	2.72	2.04			5	5	5	15	0.64	1.40	\$340,800	\$561,120		
3	Newcomb Lane	3.50	6.22	3.50			5	5	5	15		3.50	\$756,000	\$1,317,120		
3	North Creek Road	4.57	10.79	4.57			5	5	5	15		4.57	\$987,120	\$2,304,240		
3	Fallon Road	2.08	12.87	2.08			5	5	4	14		2.08	\$449,280	\$2,753,520		
2	Island Lake Road	1.28	14.15	1.28			4	5	4	13		1.28	\$121,600	\$2,875,120		
2	High-Fishtrap Lake Road	1.10	15.25		1.10		5	5	3	13		1.10	\$356,400	\$3,231,520		
4	Big Muskellunge Road	2.70	17.95	1.20	1.00	0.50	5	3	4	12		2.20	\$167,000	\$3,398,520		
2	Kern Lane	1.10	19.05		1.10		3	4	5	12		1.10	\$104,500	\$3,503,020		
4	Airport Road (paved)	2.45	21.50		1.45		5	5	2	12		0.00	\$0	\$3,503,020		
	20%															\$3,503,020
2	High Lake Road	3.80	26.50		1.30		3	3	5	11		1.10	0.70	\$331,300	\$3,834,320	
2	Shamrock Lane	0.40	21.90		0.40		3	4	4	11		0.40		\$38,000	\$3,872,320	
2	Concora Road	0.80	22.70		0.80		3	3	5	11		0.80		\$76,000	\$3,948,320	
2	Old K Road	2.79	29.29		2.79		3	3	5	11		2.79		\$397,575	\$4,345,895	
3	Dairymens Road	1.55	30.84		1.55		4	5	2	11		1.55		\$502,200	\$4,848,095	
1	Oswego-Fishtrap Lake Road	1.90	32.74		1.90		4	5	2	11		0.00		\$0	\$4,848,095	
1	South Town Line	1.36	34.10		1.36		4	5	2	11		0.00		\$0	\$4,848,095	
2	Bakken Road (East)	1.07	35.17		1.07		3	3	4	10		1.07		\$101,650	\$4,949,745	
3	Highland Drive	0.28	35.45		0.28		3	3	4	10		0.28		\$60,480	\$5,010,225	
2	Rudolph Lake Lane	0.60	36.05		0.60		3	4	3	10		0.60		\$57,000	\$5,067,225	
2	Wool Lake Lane	0.76	36.81		0.76		3	3	4	10		0.76	11.70	\$77,200	\$5,139,425	
1	North Town Line Rd (Town Road 116)	1.00	37.81		1.00		4	4	2	10		0.00		\$0	\$5,139,425	
	20%															\$1,636,405

THIS SLIDE SHOWS HOW THE SURVEY WAS EXPANDED FROM A DOCUMENT SHOWING PRIORITY RANKINGS TO ONE THAT ALSO SHOWS THE ESTIMATED COSTS ASSOCIATED WITH RECONSTRUCTING EACH ROAD BASED UPON MATERIAL TYPE.

IT IS IMPORTANT TO REITERATE, THESE ARE COST ESTIMATES, NOT FINAL COSTS.

THE SCREEN SHOT I WILL BE USING TO DEMONSTRATE HOW THE COSTS WERE CALCULATED AND HOW THEY CAN BE UTILIZED IS FROM THE FIRST SCENARIO. REMEMBER, THIS IS ONLY ONE OF THE SEVEN SCENARIOS CURRENTLY BEING EVALUATED.

USING 2017 INDUSTRY ESTIMATED PAVING COSTS PER MILE, WE WERE ABLE TO CALCULATE THE ESTIMATED RECONSTRUCTION COST OF EVERY ROAD.

FOR EXAMPLE, THE ESTIMATED COST OF UPGRADING ALL 4.57 MILES OF NORTH CREEK ROAD AT A COST OF \$216,000 / MILE WOULD COST \$987,000.

SIMILARLY, KERN LANE, AT 1.1 MILES WOULD REMAIN A CHIP SEAL ROAD AND BE RECONSTRUCTED AT A COST OF \$95,000 PER MILE OR A TOTAL OF \$104,500.

IN ADDITION, THE WORKSHEET CAN BE FURTHER SORTED BY COST SEGMENTS.

FOR EXAMPLE THE FIRST 20% OF THE ROADS, OR 19 MILES, COVERS ALL ROADS WITH A PRIORITY RANKING SCORE OF BETWEEN 12 AND 15,

THE TOTAL COST ASSOCIATED WITH THE RECONSTRUCTION PLAN FOR THESE NINE ROADS IS \$3.5 MILLION.

WHEN YOU ADD THE NEXT 20% SEGMENT OF TOWN ROADS, OR AN ADDITIONAL 13 MILES, THE PLAN COVERS 50% OF THE TOTAL ROADS TO BE IMPROVE AND JUST ABOUT ALL THE ROADS WITH A PRIORITY RANKING OF 10 AND ABOVE.

## **PUBLIC INFORMATION MEETING RECAP**

- **Process – “the funnel”**
- **Road Survey**
  - Priority Ranking
  - Cost Estimate
    - Material Upgrades
    - Paving Cost Matrix
- **Scenarios**
  - Investment Costs
  - Tax Impact
  - 90 / 10 Split
  - 2017 Interest Rates

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BEFORE MOVING INTO A REVIEW OF THE SCENARIOS, I'D LIKE TO DO A QUICK RECAP.

WE ARE CURRENTLY ONE THIRD OF THE WAY THROUGH THE PROCESS WE'RE FOLLOWING TO FINALIZE OUR ROAD PLAN RECOMMENDATIONS

THE ROAD SURVEY IS THE CRITICAL DOCUMENT USED TO CREATE THE SCENARIOS WE ARE ABOUT TO REVIEW. IT PROVIDED US WITH A PRIORITIZED LIST OF ROAD AND A COST ESTIMATE ASSOCIATED WITH RECONSTRUCTING EACH ROAD IN THE PLAN.

### **CLICK**

THE SCENARIOS TO FOLLOW REPRESENT THE INVESTMENT COST, OR THE AMOUNT OF MONEY REQUIRED TO BE ADDED TO THE BUDGET TO FUND THE IMPROVEMENT PLAN.

EACH SCENARIO WILL ALSO INCLUDE THE TAX IMPACT EACH INVESTMENT LEVEL HAS ON A COST / \$100,000 OF ASSESSED VALUATION.

EACH SCENARIO REPRESENTS A 90 / 10 SPLIT BETWEEN THE AMOUNT OF MONEY TO BE BORROWED AND THE AMOUNT OF MONEY TO BE OBTAINED THROUGH GRANTS. THEY ALL ALSO UTILIZE TODAY'S INTEREST RATES.

### **CLICK**

# Project Scenarios

## Scenario I – All Roads with Select Roads Upgraded to Asphalt

- **\$7.0 Million Investment**
- **All (64 Miles) Of Roads – 2018 & 2019**
- **Includes Road Maintenance Budget Increase - 2028**

\$7.0 Million Bond \$479,205 Average Principle and Interest 2018 - 2038	2016-2017 Average Assessed Value	2018-2028		2028-2038	
		Average Tax Impact		Average Tax Impact	
		\$101		\$171	
The average off water (416) home value is:	\$150,700	\$152		\$258	
The average waterfront (792) home value is:	\$412,300	\$416		\$705	
The average home value of (1,208) all homes is:	\$322,200	\$325		\$551	

Tax Implications and Assessment Information Provided by Paul Carlson

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Scenario i includes all 64 miles of the roads requiring reconstruction with the assumption that key roads will be able to be upgraded to asphalt. (Remember the category 3 roads highlighted in blue?)

I am going to go through this first scenario in detail to make sure you understand its contents because all seven scenarios follow the same format.

Each scenario is accompanied by the tax impact chart showing first the impact per \$100,000 in assessed valuation, and then what the tax impact would be on the average home either off water, on water or the overall average.

If we were to do all 64 miles of roads, it would required a \$7.0 million investment including funding for an increase in the road maintenance budget starting in year 2028. The targeted completion date would be in 2018 and 2019.

The annual principle and interest associated with borrowing \$7 million would be an average of \$479,205 per year over a 20 year term expiring in 2038.

The estimated tax impact would be an average of \$101 per \$100,000 in assessed property valuation for the first 10 year increasing to an average of \$171 per year in the last 10 years. The reason for the increase in the last 10 years is to cover the cost associated with increasing the current road maintenance budget by \$300,000 per year starting in 2028.

The estimated tax impact on an average off water home valued at \$150,700 would be an average of \$152 per year for the first 10 years and an average of \$256 per year for the second 10 years.

The estimated tax impact on an average on water home valued at \$412,300 would be an average of \$416 per year for the first 10 years and an average of \$701 per year for the second 10 years.

The estimated impact on the overall average home in boulder junction valued at \$322,200 would be an average of \$312 per year for the first 10 years and an average of \$548 per year for the second 10 years.

Everyone follow? It is important that you do as all the following scenarios follow a similar format.

## Project Scenarios

### Scenario II – All Roads, No Asphalt Upgrades

(Utilize Chip Seal or Stone Mastic Overlay)

- **\$5.5 Million Investment**
- **All (64 Miles) Of Roads – 2018 & 2019**
- **Includes Road Maintenance Budget Increase - 2028**

\$5.5 Million Bond \$377,423 Average Principle and Interest 2018 - 2038	2016-2017 Average Assessed Value	2018-2028 Average Tax Impact	2028-2038 Average Tax Impact
		\$79	\$148
The average off water (416) home value is:	\$150,700	\$119	\$223
The average waterfront (792) home value is:	\$412,300	\$326	\$611
The average home value of (1,208) all homes is:	\$322,200	\$255	\$477

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SCENARIO II IS THE SAME AS SCENARIO I EXCEPT ALL THE ASPHALT UPGRADES HAVE BEEN ELIMINATED AND WE WOULD USE EITHER CHIP SEAL OR STONE MASTIC OVERLAY AS THE PREFERRED SURFACE MATERIALS.

ALL THE OTHER ASSUMPTIONS REMAIN THE SAME WITH THE TOTAL INVESTMENT COST DROPPING FROM \$7.0 MILLION TO \$5.5 MILLION.

THE AVERAGE PRINCIPLE AND INTEREST COSTS DROP TO \$377,423, RESULTING IN AN ESTIMATED TAX INCREASE OF AN AVERAGE OF \$79 PER \$100,000 ASSESSED VALUATION FOR THE FIRST 10 YEARS AND AN AVERAGE OF \$148 PER \$100,000 ASSESSED VALUATION IN THE SECOND 10 YEARS.

FROM THE CHART YOU CAN SEE THE IMPACT ON THE AVERAGE HOME, EITHER OFF WATER, ON WATER OR IN GENERAL JUST AS IN THE PREVIOUS EXAMPLE AND IN ORDER TO SAVE SOME TIME I WILL NOT BE COVERING THOSE DETAILS IN THE SCENARIOS THAT REMAIN.

## Survey Overview

### Scenario III – “Top Ten” Upgraded to Asphalt

- **\$3.0 Million Investment**
- **Top Ten (20 Miles) Of Roads – 2018**
- **Includes Road Maintenance Budget Increase - 2028**

\$3.0 Million Bond \$209,083 Average Principle and Interest 2018 - 2038	2016-2017 Average Assessed Value	2018-2028 Average Tax Impact	2028-2038 Average Tax Impact
		\$44	\$113
The average off water (416) home value is:	\$150,700	\$66	\$170
The average waterfront (792) home value is:	\$412,300	\$181	\$465
The average home value of (1,208) all homes is:	\$322,200	\$141	\$364

Tax Implications and Assessment Information Provided by Paul Carlson

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SCENARIO III REDUCES THE SCOPE OF THE PROJECT TO INCLUDE ONLY THE “TOP TEN” ROADS OR ROUGHLY 20 MILES OF THE TOTAL 64 MILES IN THE PLAN.

THE INVESTMENT COST DROPS TO \$3.0 MILLION, AND ONCE AGAIN INCLUDES AN INCREASE IN THE ROAD MAINTENANCE BUDGET OF \$300,000 IN YEAR 2028.

THE ESTIMATED TAX IMPACT DROPS TO AN AVERAGE OF \$44 PER \$100,000 OF ASSESSED VALUATION IN THE FIRST 10 YEARS AND AN AVERAGE OF \$113 PER \$100,000 OF ASSESSED VALUATION IN SECOND 10 YEARS.

## Project Scenarios

### Scenario IV – “Top Ten” – No Asphalt Upgrades

Chip Seal or Stone Mastic Overlay

- **\$2.0 Million Investment**
- **Top Ten (20 Miles) Of Roads – 2018**
- **Includes Road Maintenance Budget Increase - 2028**

\$2.0 Million Bond \$137,660 Average Principle and Interest 2018 - 2038	2016-2017 Average Assessed Value	2018-2028 Average Tax Impact	2028-2038 Average Tax Impact
The average off water (416) home value is:	\$150,700	\$43	\$147
The average waterfront (792) home value is:	\$412,300	\$119	\$403
The average home value of (1,208) all homes is:	\$322,200	\$93	\$315

Tax Implications and Assessment Information Provided by Paul Carlson

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SCENARIO IV IS THE SAME AS SCENARIO III AS IT REDUCES THE SCOPE OF THE PROJECT TO INCLUDE ONLY THE “TOP TEN” ROADS OR ROUGHLY 20 MILES OF THE TOTAL 64 MILES IN THE PLAN BUT ALSO ELIMINATES THE ASPHALT UPGRADES.

THE INVESTMENT COST DROPS TO \$2.0 MILLION, AND ONCE AGAIN INCLUDES AN INCREASE IN THE ROAD MAINTENANCE BUDGET OF \$300,000 PER YEAR BEGINNING IN YEAR 2028.

THE ESTIMATED TAX IMPACT DROPS TO AN AVERAGE OF \$29 PER \$100,000 OF ASSESSED VALUATION IN THE FIRST 10 YEARS AND AN AVERAGE OF \$98 PER \$100,000 OF ASSESSED VALUATION IN SECOND 10 YEARS.

## Project Scenarios

### Scenario V – Staggered Investment

\$3.0M in 2018 (20 miles),  
 \$2.0M in 2023 (second 20 miles),  
 \$2.0M in 2018 (the last 20 miles)

- Includes Road Maintenance Budget Increase - 2028

\$3.0 Million Bond in 2018 \$2.0 Million Bond in 2023 \$2.0 Million Bond in 2028 \$161,428 Average Principle and Interest 2018 - 2038	2016-2017	2018-2023	2023-2028	2028-2038
	Average Assessed Value	Average Tax Impact \$44	Average Tax Impact \$73	Average Tax Impact \$171
The average off water (416) home value is:	\$150,700	\$66	\$110	\$258
The average waterfront (792) home value is:	\$412,300	\$181	\$301	\$705
The average home value of (1,208) all homes is:	\$322,200	\$142	\$235	\$551

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SCENARIO V PORTRAYS THE IMPACT OF A STAGGERED INVESTMENT.

IT ASSUMES THE SAME PARAMETERS OF SCENARIO I SO IT INCLUDES THE PROPOSED UPGRADES TO ASPHALT AND THE INCREASE IN THE ROAD MAINTENANCE BUDGET TO PROTECT THE INVESTMENT...

BUT IT STAGGERS THE INVESTMENT INTO FIVE YEAR INCREMENTS. \$3.0 MILLION INVESTED IN 2018, \$2.0 MILLION INVESTED IN 2023 AND THE LAST \$2.0 MILLION INVESTED IN 2028.

THE AVERAGE PRINCIPLE AN INTEREST PAYMENT IS \$161,428 PER YEAR. (JUST A WORD OF CAUTION, THIS MAY BE MISLEADING AVERAGE AS IT IS DIFFICULT TO PREDICT WHAT THE PREVAILING INTEREST RATES WILL BE IN THE YEARS 2023 AND 2028. FOR THIS EXAMPLE THE SAME INTEREST RATE WAS USED IN EACH OF THE THREE INVESTMENT LEVELS USED IN THIS EXAMPLE.)

THE ESTIMATED TAX IMPACT IS AN AVERAGE OF \$44 FOR THE FIRST FIVE YEARS, \$73 FOR THE SECOND FIVE YEARS AND \$171 FOR THE LAST 10 YEARS.

## Project Scenarios

### Scenario VI - \$300K Annual Budget Increase

- **Estimated 5 miles Reconstructed Per Year**
- **Subject To Long Term Higher Costs (Inflation Factor)**
- **Does Not Include Maintenance Budget Increase**

\$300,000 Added Annually Average Plus 2% Annual Inflation 2018 - 2038	2016-2017 Average Assessed Value	2018-2028 Average Tax Impact	2028-2038 Average Tax Impact
The average off water (416) home value is:	\$150,700	\$104	\$128
The average waterfront (792) home value is:	\$412,300	\$284	\$350
The average home value of (1,208) all homes is:	\$322,200	\$222	\$274

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SCENARIO VI ABANDONS ANY THOUGHT OF BONDING AND JUST LOOKS AT INCREASING THE CURRENT ROAD RECONSTRUCTION BUDGET BY \$300,000 PER YEARS.

THE INCREASE WOULD RESULT IN AN AVERAGE BUDGET OF \$550,000 IN TOTAL FROM WHICH WE ESTIMATED AN AVERAGE OF 5 MILES OF ROADS COULD BE IMPROVED PER YEAR.

THIS IS VERY SIMILAR TO THE PROPOSAL THAT WAS MADE IN 2016 BUT REPEATED EVERY YEAR FOR THE NEXT 20 YEARS.

THE ESTIMATED TAX IMPACT WOULD BE AN AVERAGE OF \$69 PER YEAR FOR THE NEXT 10 YEARS AND AN AVERAGE OF \$85 PER YEAR FOR THE SECOND 10 YEARS REFLECTING THE ESTIMATED IMPACTS OF INFLATION.

IT DOES NOT INCLUDE ANY INCREASE IN THE ANNUAL ROAD MAINTENANCE BUDGET.



# Project Scenarios

## Scenario VII - \$200K "Emergency" Budget Increase

- Only "Failed" Roads Get Addressed
- Subject To Long Term Higher Costs (Inflation Factor)
- Does Not Include Maintenance Budget Increase

\$200,000 Added Every Other Year Plus 3% Inflation Every 2 years 2018 - 2038	2016-2017 Average Assessed Value	2018-2028 Average Tax Impact	2028-2038 Average Tax Impact
The average off water (416) home value is:	\$150,700	\$68	\$78
The average waterfront (792) home value is:	\$412,300	\$186	\$214
The average home value of (1,208) all homes is:	\$322,200	\$145	\$168

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SCENARIO VII IS A "FIX AS FAIL" APPROACH WHICH MEANS WE WOULD ONLY INCREASE THE BUDGET WHEN A ROAD FAILS OR IS DEEMED UNSAFE FOR PASSAGE.

THIS SCENARIOS ASSUMES A \$200,000 EMERGENCY FUND IS REQUIRED EVERY OTHER YEAR RESULTING IN AN ESTIMATED TAX INCREASE OF \$45 PER YEAR FOR THE FIRST 10 YEARS AND \$52 PER YEAR FOR THE SECOND 10 YEARS ONCE AGAIN REFLECTING THE IMPACTS OF INFLATION.

LIKE SCENARIO VI, IT ALSO DOES NOT INCLUDE ANY INCREASE IN THE ANNUAL ROAD MAINTENANCE BUDGET.

## Project Scenario Summary

- Higher Investment = Higher cost, highest miles of coverage, fastest implementation, and everyone benefits quickly
- Lower Investment = Lower cost, least miles of coverage, slowest implementation and very few benefit quickly

	Investment Amount	Roads Reconditioned	Years to Completion	Tax Impact 2018 - 2028	Tax Impact 2028 - 2038
SCENARIO I Complete All Roads As Proposed With Asphalt Upgrades	\$7,000,000	64	2	\$101	\$171
SCENARIO II Complete All Roads (Asphalt Upgrades Eliminated)	\$5,500,000	64	2	\$79	\$148
SCENARIO III Top 10 Roads (19.05 miles) As Planned With Asphalt Upgrades	\$3,000,000	20	1	\$44	\$113
SCENARIO IV Top 10 Roads (19.05 miles) Asphalt Upgrades Eliminated	\$2,000,000	20	1	\$29	\$98
SCENARIO V Staggered Investment, \$3m in 2018, \$2M in 2023 and \$2M in 2028	\$7,000,000	64	10	\$73	\$171
SCENARIO VI \$300,000 Annual Budget Increase	\$300,000	5	20	\$69	\$85
SCENARIO VII \$200,000 "Emergency" Budget Increase	\$200,000	0 - 4	20	\$45	\$52

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### IN SUMMARY:

Higher Investment = Higher cost, highest miles of coverage, fastest implementation, and everyone benefits quickly

Lower Investment = Lower cost, least miles of coverage, slowest implementation and very few benefit quickly

HERE IS A QUICK RECAP.

READ THROUGH EACH SCENARIO.

IN SUMMARY, WE AS A COMMITTEE HAVE NOT FOUND A SCENARIO TO BE PURSUED WHERE THERE WILL NOT BE THE POTENTIAL FOR THE NEED FOR INCREASED FUNDING FOR ROADS RESULTING IN THE POTENTIAL FOR A TAX INCREASE. ROAD IMPROVEMENT NEEDS WILL ALWAYS BE THERE, IT WILL JUST BE A MATTER OF HOW AND WHEN WE ARE GOING TO HAVE TO FUND IT.

BEFORE MOVING ON, DO A PROCESS CHECK. EVERYONE STILL WITH ME?

LAST BUT NOT LEAST, NEXT IS A QUICK REVIEW OF THE SCHEDULE MOVING FORWARD AND THEN THE Q&A

## Project Milestones

### APRIL

- **Conduct two public information meetings:**
  - **April 12, 2017 6:30 PM to 8:30 PM**
  - **April 13, 2017 10:00 AM to 12:00 PM**
- **Conduct Meeting Of Electors Update on 18 April 2017**  
One segment will be on the Road Improvement Project

### MAY

- **“Conditions Assessment” – Road by Road Review**

### JUNE & JULY

- **Conduct additional public information meetings to get input from the summer residents.**
- **Address lake associations, civic and community organizations to gain input from these key support organizations.**

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DURING APRIL WE WILL CONDUCT TWO PUBLIC INFORMATION SESSIONS AND PROVIDE AN UPDATE AT THE ANNUAL MEETING OF THE ELECTORS.

IN MAY, WE HOPE TO COMPLETE THE ALL IMPORTANT “CONDITIONS ASSESSMENT”.

IN JUNE AND JULY WE PLAN ON CONDUCTING ADDITIONAL PUBLIC INFORMATION GATHERING SESSIONS INCLUDING MEETING WITH SOME OF THE LAKE ASSOCIATIONS AND CIVIC AND COMMUNITY ORGANIZAITONS.

CLICK

## Project Milestones

### **JULY**

- **Project Committee assesses all the potential scenarios and selects one for recommendation to the Town Board**

### **JULY & AUGUST**

- **The Town Board reviews the recommendation and decides on what action is to be taken**

### **SEPTEMBER**

- **Conduct a Special Meeting Of Electors to vote on the recommendation approved by the board**

*September meeting will be critical to make 2018 budget preparation, project bidding, and 1 October grant application deadline*

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IN JULY, THE COMMITTEE WILL ASSESS ALL THE POTENTIAL SCENARIOS AND SELECT ONE FOR RECOMMENDATION TO THE TOWN BOARD.

IN JULY AND OR AUGUST THE TOWN BOARD WILL REVIEW THE RECOMMENDATION AND DECIDE ON WHAT ACTION TO TAKE.

IN SEPTEMBER, WE HOPE TO BE BACK HERE WITH YOU AGAIN TO PRESENT A RECOMMENDATION FOR YOU TO VOTE ON

LASTLY...SEPTEMBER IS CRITICAL TO MAKE THE 2018 BUDGET PREPARATION, PROJECT BIDDING AND THE 1 OCTOBER GRANT APPLICATION DEADLINE FOR 2018 FUNDING.

## Project Feedback

- **Question and Answer Session Ground Rules:**

- Ask one question at a time
- Try to ask it in 60 seconds or less
- Will repeat until everyone has a chance to speak

BEFORE I MOVE TO THE Q&A, I'D LIKE TO INTRODUCE THE OTHER MEMBERS OF THE COMMITTEE....

THE GROUND RULES FOR THIS SESSION ARE:

ASK ONE QUESTION AT A TIME

TRY TO ASK IT IN 60 SECONDS OR LESS SO WE CAN GET TO EVERYONE'S QUESTION,

WE WILL CONTINUE TO GO AROUND THE ROOM UNTIL EVERYONE HAS A CHANCE TO ASK THEIR QUESTION

## Project Breakouts

- **Information Breakout Session & Adjournment**
  - What do you like about this scenario?
  - What concerns do you have about this scenario?
  - How can we make it better?

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WE'D NOW LIKE TO BREAK FROM THE FORMAL PART OF THE MEETING TO THE INFORMAL PART.

WE HAVE ALL OF THE SCENARIOS WE COVERED DURING THE MEETING UP ON THE WAY AND THE MEMBERS OF THE COMMITTEE WILL BE AVAILABLE TO ANSWER ANY ADDITIONAL QUESTION YOU MAY HAVE AND ALSO TO HELP US ANSWER THE FOLLOWING QUESTIONS:

- **What do you like about this scenario?**
- **What concerns do you have about this scenario?**
- **How can we make it better?**

## Town Road Improvement Project

# THANKS

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**We thank you in advance for coming to this meeting and helping us further define our road improvement plan**